



# East Albany Plan

**ADOPTED MAY 10, 2023**



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# Executive Summary

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East Albany represents much of the Albany community's future growth and expansion. In the two decades since the original "East I-5 Plan" was prepared, the area has seen housing growth, as well as development of larger commercial businesses and light industrial facilities. However, more is needed to create the kind of community that Albany desires there in the future.

In the future, East Albany is envisioned to be a vibrant area that contains walkable and diverse residential neighborhoods and mixed-use areas with a variety of housing types for households of all incomes, significant employers, locally-serving commercial areas and regional facilities and destinations.

The area will include a network of trails to provide recreation opportunities and transportation connections for residents, visitors, and employees in the area, while a safe and accessible multi-modal network connects transit users, pedestrians, bicyclists, and automobile drivers across the area and to the rest of the City.

Its creeks, wetlands, and other natural areas are protected and enhanced through development; they serve as an open space amenity, provide critical habitat, help manage stormwater and maintain water quality, and help mitigate the impacts of climate change.

This East Albany Plan articulates that vision and the steps needed to achieve it. It describes future development of a mix of new and redeveloped areas for housing, jobs, parks, trails, schools, and other needed facilities, along with a transportation system that connects the area and allows for a full range of ways to travel.

This Plan has been prepared through a collaborative process involving City staff, a consulting team, a broad range of City advisors and decision-makers, and extensive guidance from property owners and the community at large.

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## Key Elements of the Plan



A mix of older residential neighborhoods, new single-family and multi-family homes, and mixed use areas that include a combination of housing, businesses and gathering places that serve surrounding residents and neighborhoods – in other words, a collection of varied, walkable neighborhoods oriented to adjacent or nearby shops, restaurants and other businesses.



Numerous opportunities for new jobs and existing businesses. These will include larger scale employment areas along Three Lakes Road and Highway 20, as well as mixed use centers at Timber Ridge/Highway 20, Spicer Road/Timber Street, Scrael Hill/Knox Butte, and other select locations that provide retail and services for residents and visitors.



A network of trails, pathways, and enhanced sidewalks connecting homes to nearby business, schools and park, as well to regional destinations like Timber Linn Park.



New and improved roadways that do a more effective job of managing automobile and truck traffic and also provide better opportunities for people to walk, bike, or use transit for their travel needs.



Parks and schools that will serve new residents as the area continues to grow and develop.



Improvements to city infrastructure, such as water, wastewater, and stormwater facilities, including major water and wastewater lines, reservoirs and pump stations. Plans for these facilities address new growth projected for the area, while building on the City's existing master plans for these facilities.

The remainder of this plan describes these elements in more detail, culminating in a detailed Action Plan that sets the stage for future implementation of the Plan.

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# Chapter 1

## Introduction and Context

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### **Vision Statement**

*East Albany is a vibrant part of the City that contains walkable and diverse residential neighborhoods and mixed-use areas with a variety of housing types for households of all incomes, significant employers, locally-serving commercial areas and regional facilities and destinations.*

*A network of trails and pathways provides recreation opportunities and transportation connections for residents, visitors, and employees in the area, while a safe and accessible multi-modal network connects transit users, pedestrians, bicyclists, and automobile drivers across the area and to the rest of the City.*

*East Albany's creeks, wetlands, and other natural areas are protected and enhanced through development; they serve as an open space amenity provide critical habitat, help manage stormwater and maintain water quality, and help mitigate the impacts of climate change.*

### **Planning the Future of East Albany**

East Albany has a large land base that represents much of the community's future growth and expansion. In the two decades since the original "East I-5 Plan" was prepared, the area has seen housing growth, as well as development of larger commercial businesses and light industrial facilities. However, development of neighborhood commercial services and creation of an integrated system of roads, parks, and trails has lagged.

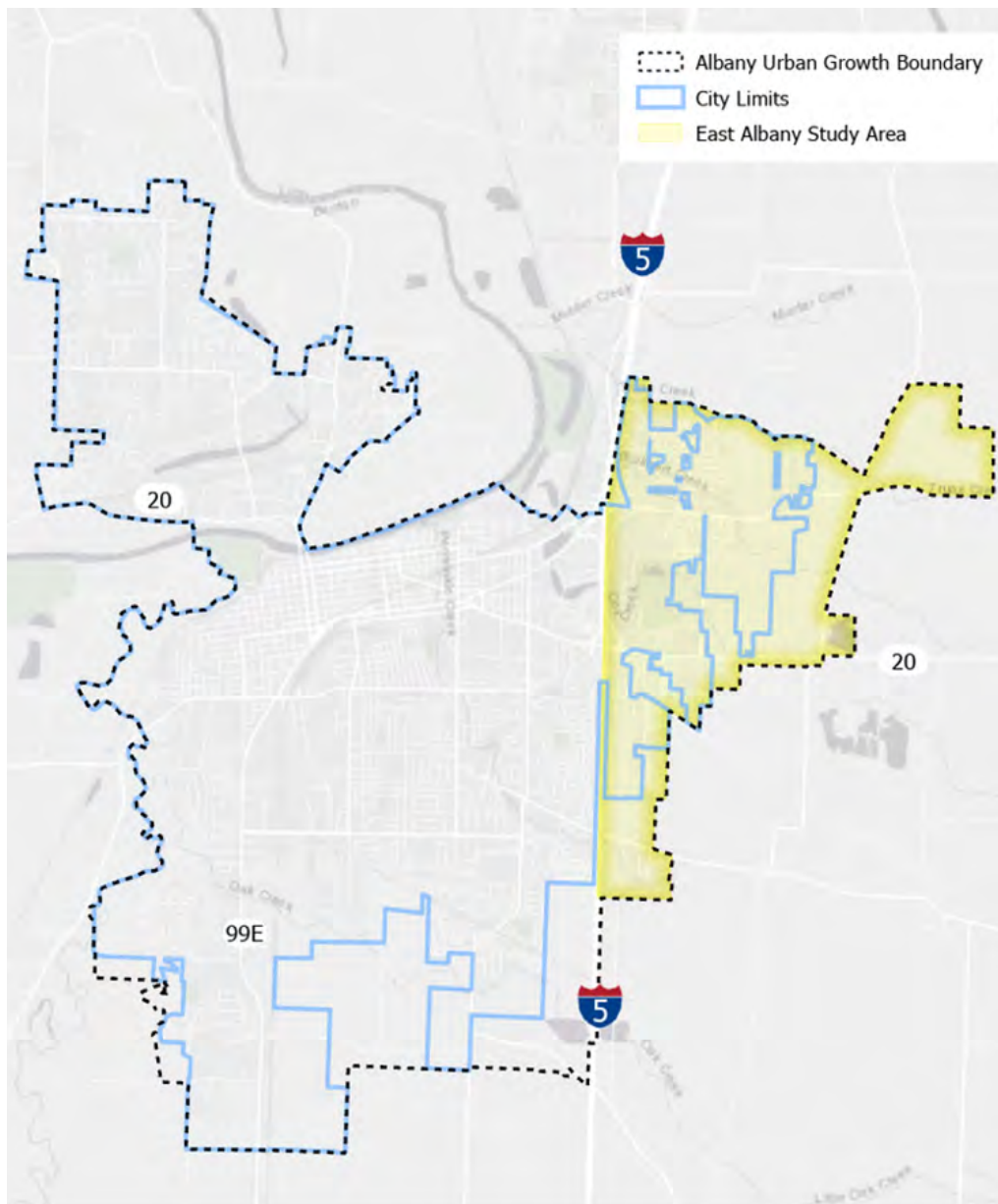
The East Albany Plan is a fresh look at this key growth area for the City that focuses on providing connectivity, transportation choices, housing variety, and vibrant mixed-use and employment centers around the East Albany's natural resources and viewsheds.



### Study Area

This project addresses the area shown in Figure 1. It includes all of the land within the Albany Urban Growth Boundary (UGB) east of Interstate 5 (I-5) and totals about 2,800 acres. The City's largest park, Timber Linn Park, the Albany Municipal Airport, and the Linn County Fair and Expo Center are located in the western portion of the Study Area near I-5. Most commercial development in East Albany is along US 20 or near I-5 and has a regional draw (Walmart, Home Depot, car sales) or caters to travelers. A collection of properties zoned Regional Commercial adjacent to the Knox Butte I-5 interchange remain underdeveloped with low-density residential uses. Recent development has included both traditional residential neighborhoods with single-family homes, as well as more recent development of apartments.

Figure 1. East Albany Area







## East Albany Plan Goals

The following goals were established by the project’s advisory committee, with input from the Albany City Council, Planning Commission and the public.

1. Establish a development pattern of diverse, connected neighborhoods, services, and amenities through an appropriate mix of zoning designations.
2. Connect residential, employment, commercial, and recreational uses with a functional, and safe and equitable multi-modal transportation system and that promotes walking, biking, and transit and supports public health and climate change goals.
3. Provide a transportation system that provides access and mobility for all modes of travel and supports anticipated residential and employment growth.
4. Support the City’s economic development and employment goals and address barriers to achieving those objectives.
5. Increase the variety of housing types, densities, and mixed-use developments to accommodate the needs of current and future residents, including housing that is affordable and accessible to all Albany residents at all income levels.



Example of a mixed-use development, featuring apartments over commercial uses and generous public spaces.



Example of a mixed-use development, featuring apartments over commercial uses



Townhouses with a public green adjacent to a community bike path



Example of an apartment development next to a restored creek and wetland corridor



Example of high-density multi-family development, enclosing a courtyard with public gathering spaces.



Example of a triplex, a residential pattern called "Middle Housing" that is increasingly popular



Example of an apartment development with a landscaped courtyard



Smaller scale retail and dining uses with outdoor seating



This example in Hillboro, shows a mixed-use "main street" situated perpendicular to a busy arterial (like Highway 20). Parking is placed behind buildings, and density transitions down from this main street towards sin-

**East Albany will contain a range of existing and new neighborhoods, with a variety of housing types.**



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6. Protect and improve the area’s watersheds, riparian and wildlife corridors, wetlands and drainage ways as habitat areas and for stormwater functions to help mitigate and adapt to climate change.
7. Use interconnected natural resource corridors and greenway trail system to provide pedestrian, open space, and wildlife connectivity between neighborhoods and to help protect scenic vistas and rural character.
8. Identify opportunities to provide and enhance aesthetic, recreational, and open-space amenities, including existing and new parks in the area. Multi-modal access to Timber Linn Park is a key opportunity for this plan.
9. Create attractive gateways into Albany from I-5 and US 20.
10. Work with local and regional government partnering agencies to implement a coordinated approach to future annexation and development within the area and ensure that public facilities and infrastructure are sized and designed to serve planned levels of development.
11. Identify a strategy for implementation, including short-term and long-term measures and funding sources the City can take that will help achieve its goals for Project Study Area.



Flexible light industrial buildings can accommodate a wide range of businesses at a variety of scales



Light industrial and office building in East Albany study area



Corporate campus with gathering spaces as amenities for employees



Flexible warehouse with stormwater planters and bike infrastructure integrated in parking areas



Employment areas should include sidewalks to enable employees to walk to work and generous landscape



Light industrial and office facilities can take advantage of the natural creek corridors and water features in East Albany

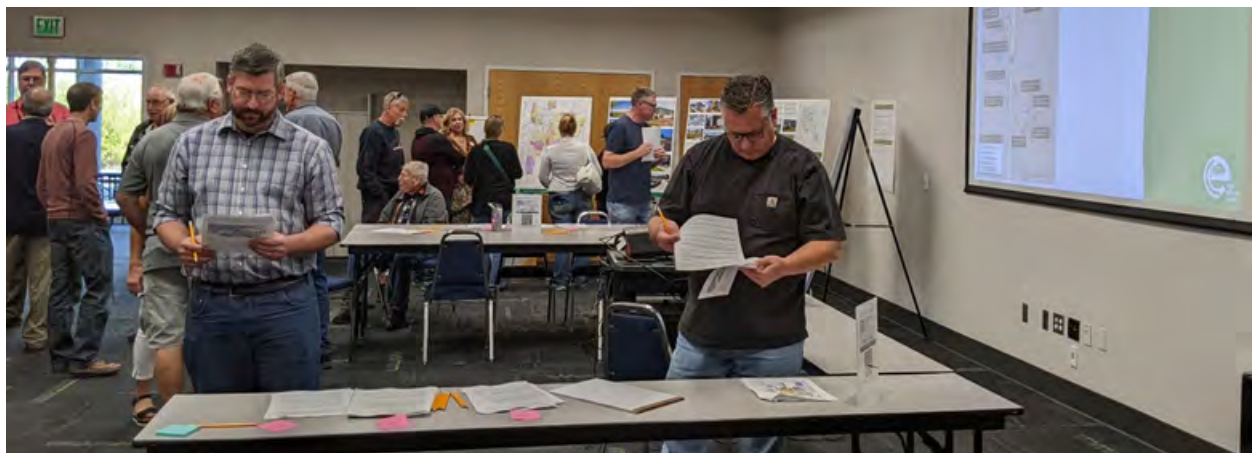
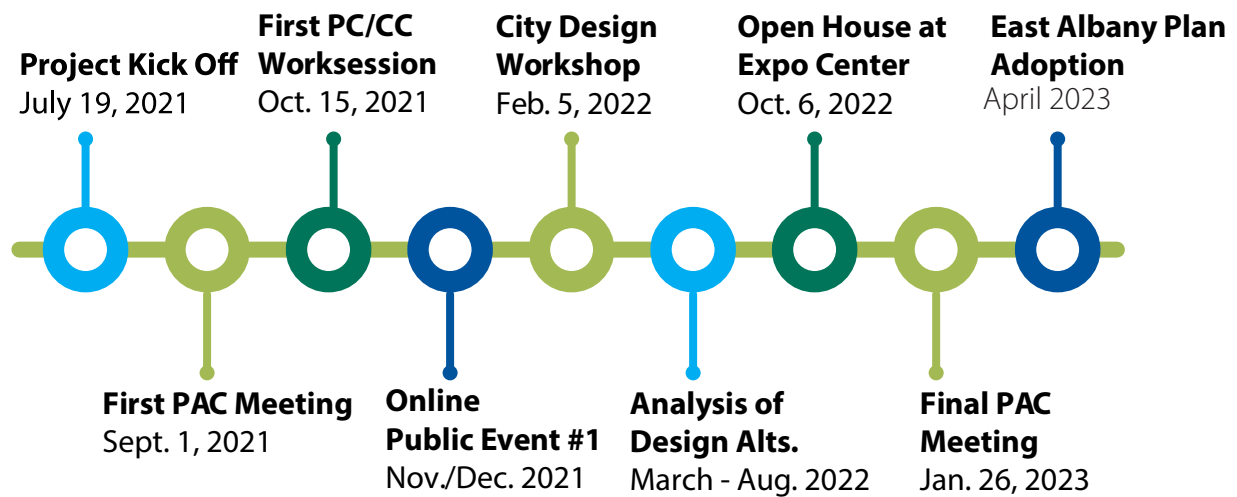


# Chapter 2 Planning Process

## Process

This plan was developed over nearly two years through the process described in Figure 2. Phase 1 established the overarching vision and goals for the East Albany area, Phase 2 analyzed various alternatives for land uses and transportation options in the area, and Phase 3 included refinements and detailed planning for the preferred land use alternative.

Figure 2. East Albany Plan Process Diagram



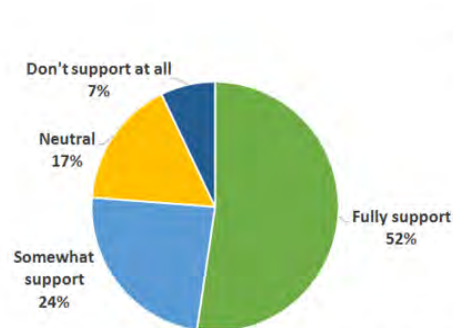
Attendees at the second open house provided feedback on land use scenarios.



## Community Engagement

In preparing this Plan, the City and its consulting team implemented a robust community engagement program. Activities included the following:

- **Focus group meetings.** The City conducted three sets of focus group meetings with residents, property owners, businesses, and community leaders at key project milestones. These meetings were instrumental in identifying key planning issues and providing feedback on initial planning concepts.
- **Project Advisory Committee (PAC).** The City appointed a technical committee with representation from Greater Albany Public Schools, Albany Bike/Pedestrian Committee, Cascades West Council of Governments, Oregon Department of Transportation, Linn County, real estate, property owners, and City public works and parks and recreation departments. Six PAC meetings were held throughout the various phases of the project. The PAC’s role was to review all project deliverables and community input and provide guidance to the city and consultants.
- **One-on-one meetings with businesses and property owners.** The City met with a variety of business and property owners to consider their concerns and suggestions regarding recommendations related to specific individual properties and improvements.

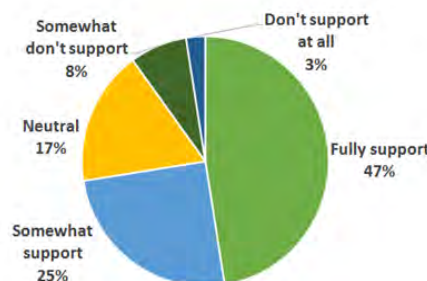


What is your level of support for the East Albany vision?



Left: Input on the draft project vision and goals from the first open house.

What is your level of support for the project goals?



Above: Mayor’s welcome message from the Online Open House.



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- **Online and In Person Open Houses.** The first round of public engagement for the East Albany Plan included an online and in person open houses at City Hall to review the project's overall vision and goals, and visual character of potential development types. The second round of public engagement included an online survey and a community open house to solicit comments on specific elements of the land use and transportation plans.
- **Community Conversation.** City staff hosted an open conversation with area residents at Cascade Grill in East Albany.
- **Project Webpage and Social Media.** The team created a Project Webpage at the outset of the project to provide information about the planning process; access to draft reports, memos, maps and other materials; announcements about upcoming community events; links to online surveys; and other opportunities to comment on the project. The City also used Facebook and other social media channels to encourage people to participate in the project and let them know about upcoming events and activities.
- **Planning Commission and City Council Briefings.** The Albany Planning Commission and City Council held four joint work sessions throughout the project to review project deliverables and community input and provide input throughout the project.
- **Interested Parties Communications List.** Throughout the project, the City created and expanded an interested parties list to keep people who expressed interest in the project up-to-date on the project's progress, additional opportunities to participate, and available project materials and recommendations.



*Open house at the Linn County Fair and Expo Center.*



# Chapter 3

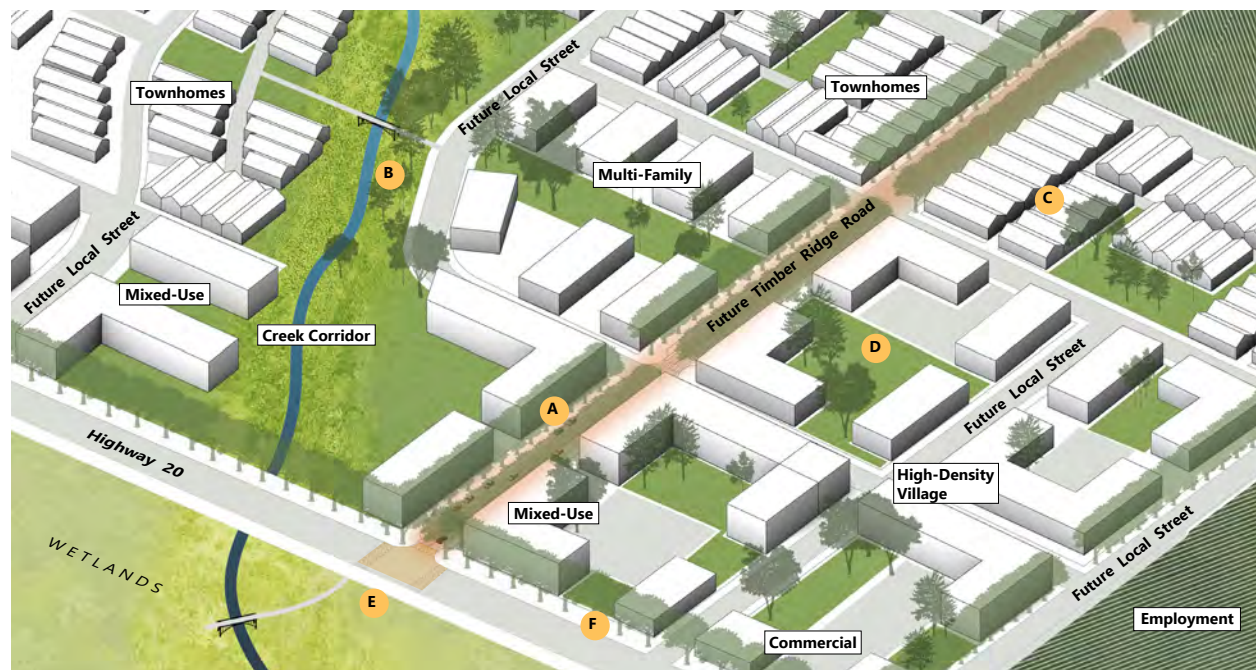
## Land Uses

### Land Use Concept

The East Albany Plan land use concept is shown in Figure 3. The concept was developed with input from the public, property owners, and City staff and decisionmakers and underwent an extensive, iterative process of developing initial scenarios, evaluating them, selecting a preferred plan and then identifying several rounds of refinement. The proposed land use plan includes:

- Mixed use neighborhood and village centers along corridors to connect residents with services, shopping, and activities.
- A connected greenway system providing multi-modal transportation between neighborhoods and employment.
- Significant employment areas along Highway 20 and in the southern portion of the East Albany area.
- Retains low density residential areas and adds opportunities for medium and higher density residential uses near mixed use, commercial and employment land.
- Adds commercial, residential, and mixed use opportunities near Spicer Road and Goldfish Farm Road.

Figure 3. Conceptual diagram of residences on a Main Street



- A** Mixed-use 'Main Street' with wide sidewalks, on-street parking and tree canopy
- B** Greenway with restored creek, multi-use trails, bridge crossings and public street frontage
- C** Density Transitions away from Mixed Use Main Street
- D** Courtyards for gathering spaces, landscape and parking (reduced in climate-friendly area)
- E** Signalized node with safe pedestrian crossings, wide sidewalks & access to wetlands
- F** Improvements to Highway 20 frontage with sidewalks and new commercial buildings





## Land Use Districts

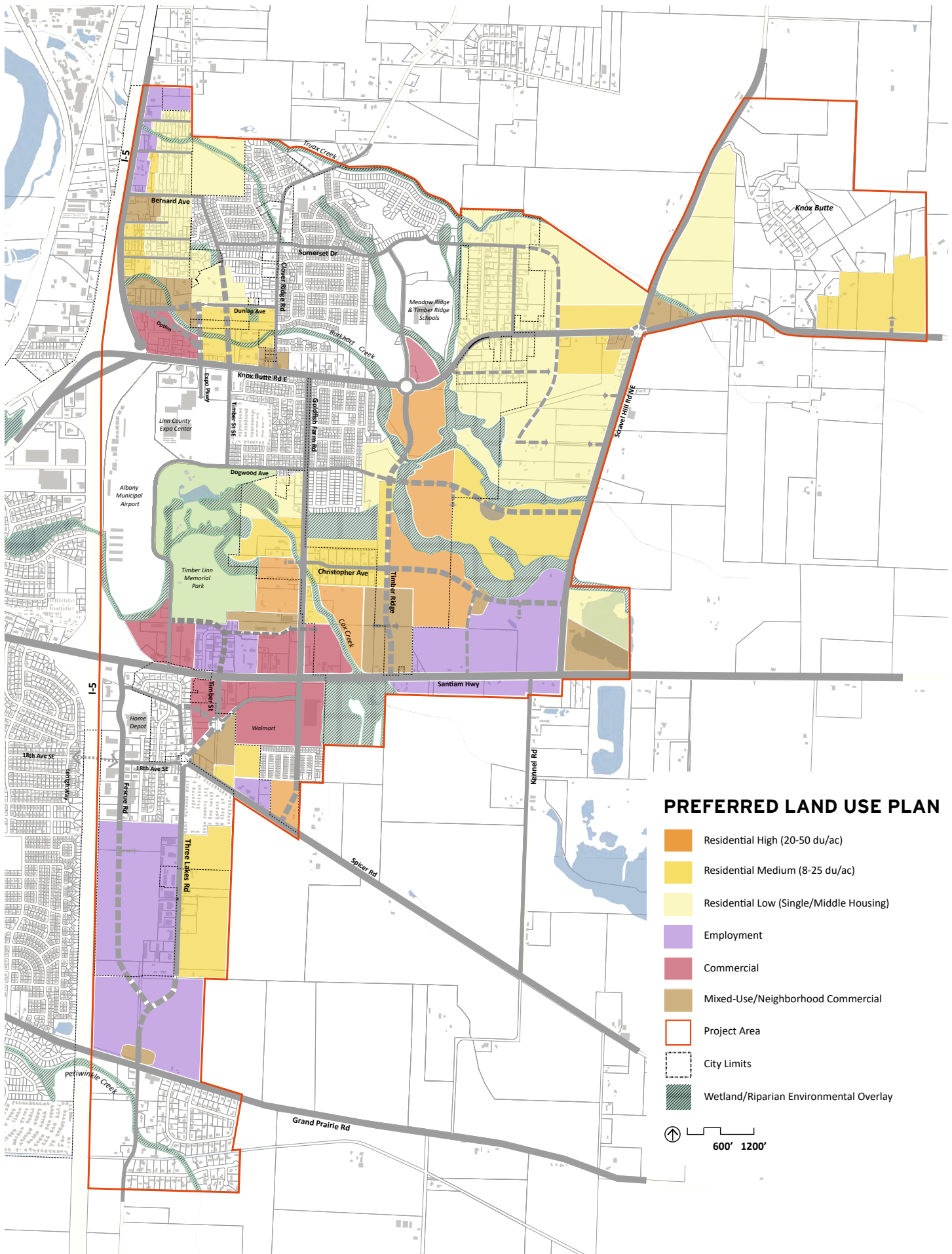
Table 1 describes the concepts shown in Figure 4, along with the implementing comprehensive plan and zoning designations and a summary of the allowed uses within those districts.

*Table 1. Description of Preferred Land Use Concept*

Land Use Concept	Comprehensive Plan Designations	Zoning Designations	Uses
<b>Residential High</b>	Residential High Density	High Density Residential (HDR), Medium Density; Attached (RMA), Mixed Use; Residential (MUR)	Residential and mixed use buildings up to 50 units per acre
<b>Residential Medium</b>	Residential Medium Density	Residential Single Dwelling Unit (RS-5), Residential Medium Density (RM), Residential Medium Density Attached (RMA), Mixed Use Residential (MUR)	Residential uses up to 35 units per acre
<b>Residential Low</b>	Residential Low Density	Residential Single Dwelling Unit (RS-6.5, RS-5)	Detached single and middle housing dwelling units
<b>Employment</b>	Employment	Employment(EMP), Industrial Park (IP), Office Professional (OP)	Manufacturing, light industrial uses, offices, limited commercial
	Light Industrial*	Light Industrial (LI)*, Industrial Park (IP)	
<b>Commercial</b>	Commercial General	Neighborhood Commercial (NC), Office Professional (OP), Community Commercial (CC), Regional Commercial (RC)	Retail and services at a range of scales.
<b>Mixed-Use/ Neighborhood Commercial</b>	Village Center – East	Mixed Use Commercial (MUC); Mixed Use Residential (MUR)	Residential, retail, office, community and personal services.
	Neighborhood Village	Neighborhood Commercial (NC), Office; Professional (OP), Mixed Use Residential (MUR)	

*\* Light Industrial - This Comprehensive Plan designation and zoning district may not be applied to properties in East Albany after July 1, 2023. Properties zoned Light Industrial on June 30, 2023, may remain zoned Light Industrial, including those designated Employment on the Comprehensive Plan map.*

Figure 4. East Albany Plan Land Use Concept



**PREFERRED LAND USE PLAN**

- Residential High (20-50 du/ac)
- Residential Medium (8-25 du/ac)
- Residential Low (Single/Middle Housing)
- Employment
- Commercial
- Mixed-Use/Neighborhood Commercial
- Project Area
- City Limits
- Wetland/Riparian Environmental Overlay



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## **Comprehensive Plan Designations**

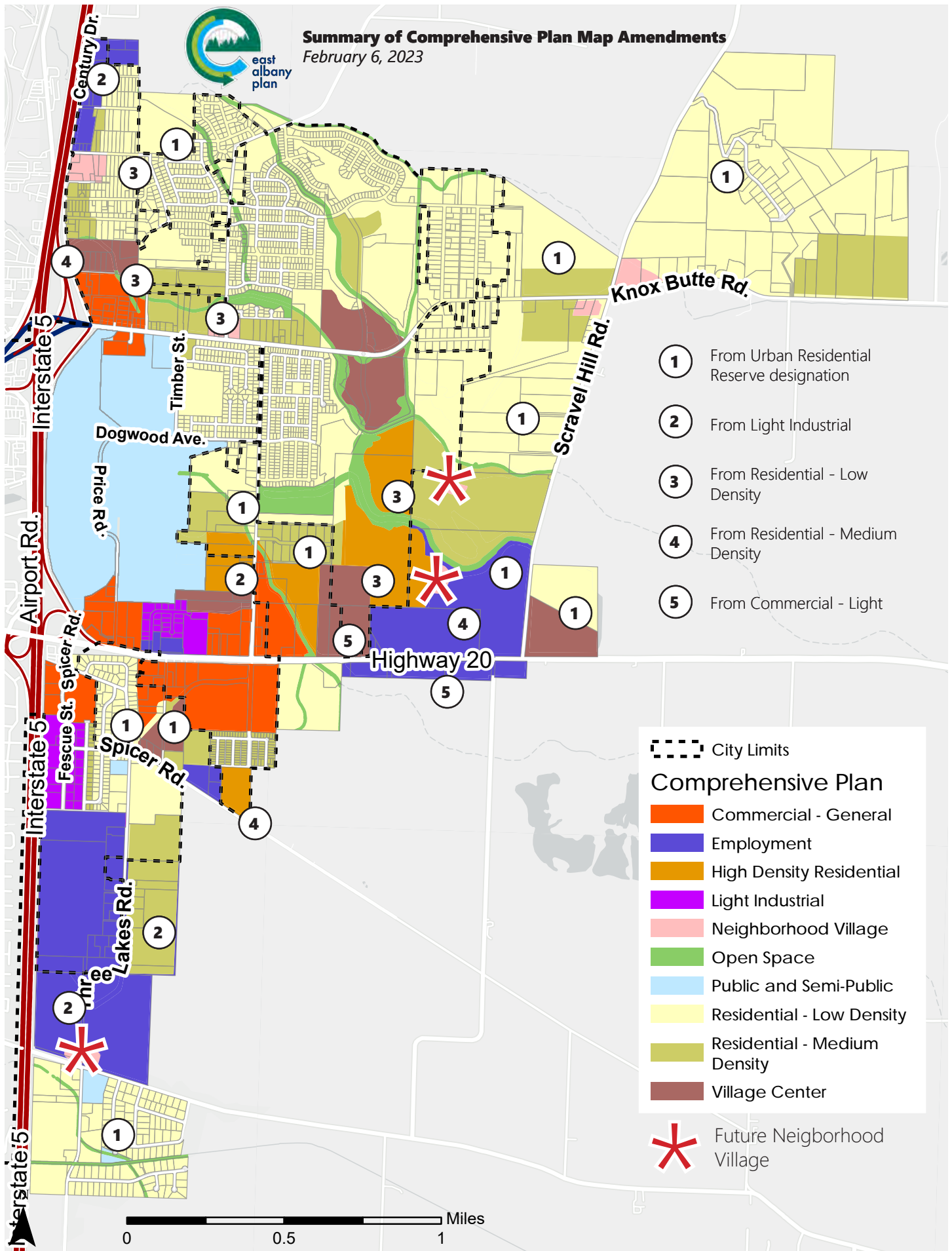
Land use within the City's UGB is subject to the City's comprehensive plan. The comprehensive plan designations that would implement the land uses proposed in the East Albany Plan are shown in Figure 5.

As part of the planning process, the City will add references to and area-specific policies for the East Albany Plan to the urbanization section of the Comprehensive Plan, along with several additional text amendments to implement the land use plan concepts. In addition, a new Employment designation is proposed and the High Density Residential designation is being revived. The Comprehensive Plan and Plan Map will be updated to include those designations.

## **Zoning**

Properties within the Albany city limits have a City zoning district designation which must be consistent with their Comprehensive Plan designation. As part of this planning effort, the City proposes to change the zoning of a few properties to be compatible with the East Albany land use plan concepts and the Comprehensive Plan designations. In addition, the new Employment and High Density Residential zones are proposed to be added to the Zoning map and the Albany Development Code. In the future, areas within the East Albany Plan area but currently outside the City limits are expected to be annexed into the City. At that time, a zoning designation would be applied to those areas, also consistent with the proposed new Comprehensive Plan designations and the East Albany land use plan. The annexation process is described further in Chapter 7.

Figure 5. Comprehensive Plan Designations





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### Residential Uses

East Albany contains a mix of older neighborhoods, new single-dwelling and multi-dwelling construction, and land planned for future residential growth. This plan aims to create a collection of varied, walkable neighborhoods around sites of commercial activity.

The greatest concentration of new residential growth will be along the extension of Timber Ridge Road, transitioning to lower intensity residential to the east and employment uses along Highway 20.



*A cottage cluster development with a gathering in the central courtyard.*



*Example of an apartment development next to a restored creek and wetland corridor.*



*Example of a triplex, a type of "Middle Housing" that is increasingly popular.*



*Townhouses with a public green space adjacent to a community bike path.*



*Example of mixed-use development with apartments over commercial uses like stores and restaurants.*



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## Employment Uses

East Albany contains a significant amount of the City's employment land, as well as sites for highway-oriented commercial and nodes of neighborhood commercial activity. Land along Three Lakes Road and along Highway 20 is identified for large scale employment uses, while mixed use nodes at Timber Ridge/Highway 20, Spicer Road/Timber Street, Scrael Hill/Knox Butte, and other select locations provide retail and services for residents and visitors.



*Light industrial and office facilities can take advantage of natural features in East Albany.*



*A 'green manufacturing' facility with careful site planning and renewable energy.*

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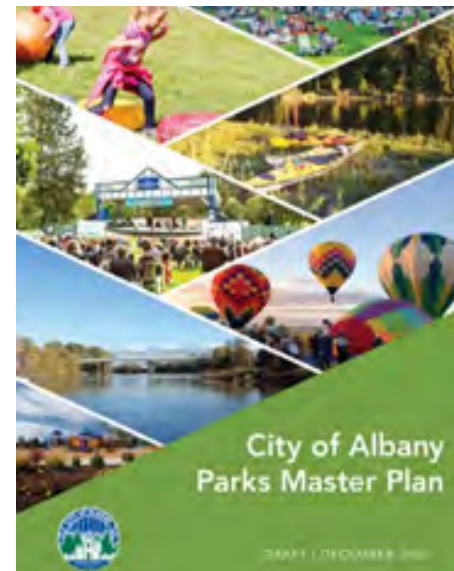
# Chapter 4

## Parks, Trails, and Open Space

The vision for East Albany includes a system of parks, trails, and open spaces connecting neighborhoods and preserving natural stream corridors. The City recently adopted a Park System Master Plan which will direct investments to implement parks goals citywide, as well as in East Albany.

### Timber Linn Park

Timber Linn Memorial Park is an exceptional resource for city and residents in East Albany. Located between the Albany Airport and Cox Creek, the park provides a large grassy amphitheater with a waterfront stage, two large covered pavilions, plenty of space for large community events. The park also provides soccer and softball fields, a playground, and Veterans' memorial. Timber Linn Dog Park was added to the northeast end of the park, close to residential developments.



### Planning for other Community Parks and Neighborhood Parks

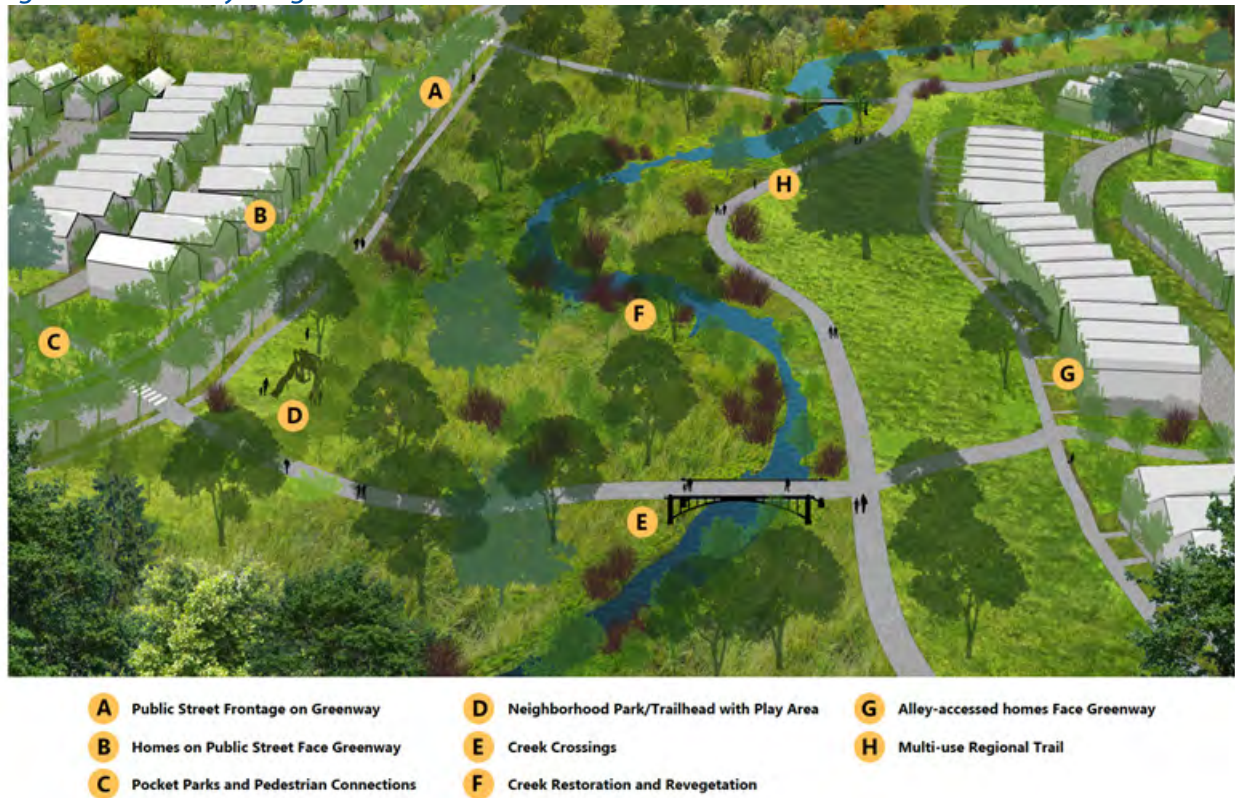
The 2021 Parks Master Plan (PMP) includes development of Timber Ridge Neighborhood Park, located between Meadow Ridge and Timber Ridge schools. The plan for the park has been developed, and the first phase construction is anticipated summer of 2023. Development of the Timber Ridge property will provide a local park with play equipment within walking distance to many new neighborhoods in East Albany.

The master plan identifies three areas East of I-5 outside the city limits for potential park locations to serve future residential growth, including a trail connecting the Timber Ridge schools to Timber Linn Park. Other park and recreation master planning priorities and goals are described later in this chapter.

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Figure 6. Greenway Diagram



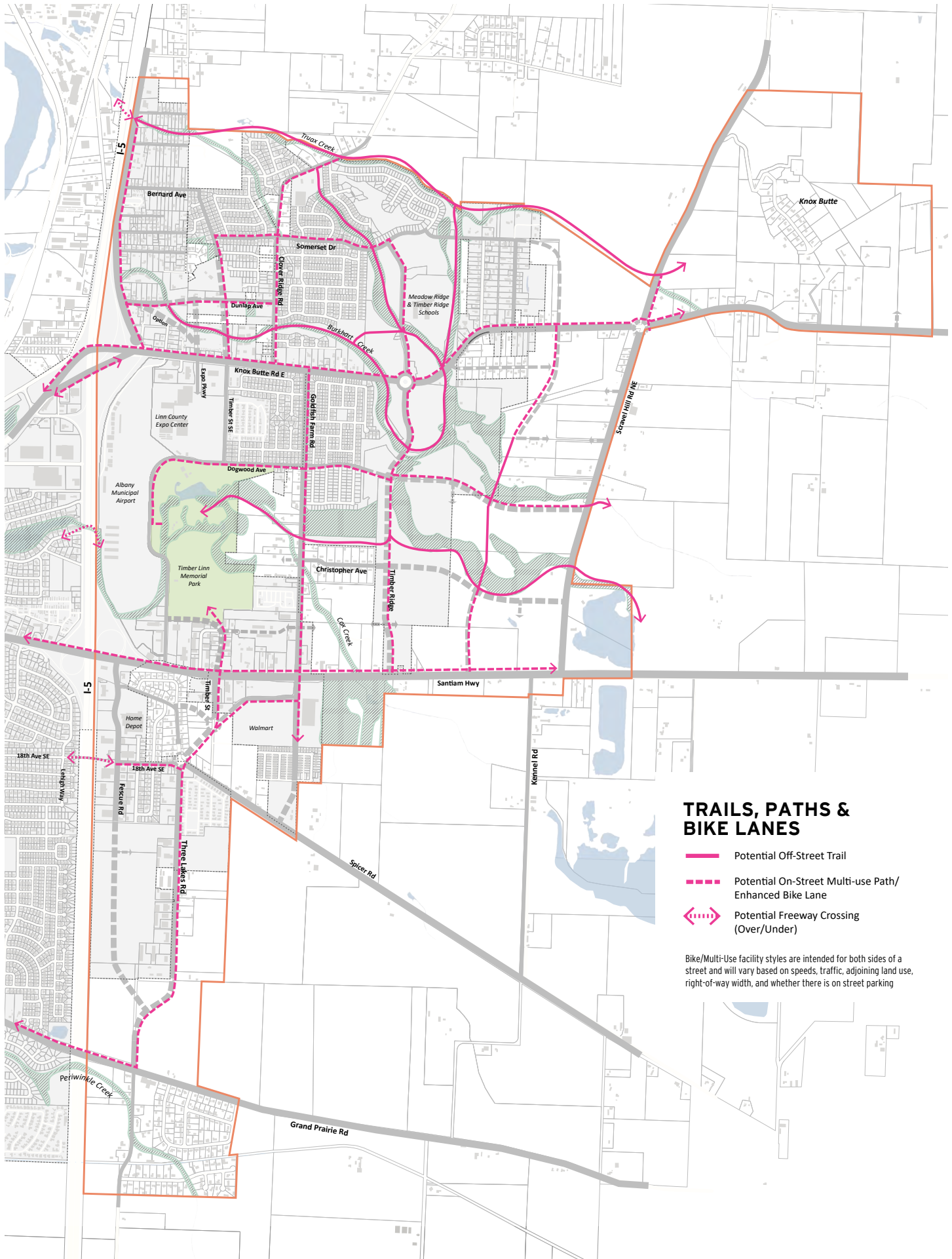
## Greenways and Trail Corridors

Public input for the Parks master plan identified the popularity of and desire for more trails and pathways as walking/biking is the top activity people would like to see more of. Building more trails and paths was ranked as the second most important funding priority by the public. The Parks plan identifies a trail and enhanced sidewalks connecting the neighborhoods to the schools to Timber Linn Park.

The study area includes a number of streams that generally flow in a northwesterly direction towards the Willamette River. Over time, these creeks have often been channelized and native vegetation has been removed through agricultural activity. But as new development occurs near these streams, protective riparian buffers will be applied, which should result in a wide, vegetated corridor that improves stream health. These corridors have great potential to enhance habitat but they can also serve as residential amenities as well as trail corridors, particularly creating connections between neighborhoods and schools (Figure 6). As new neighborhoods continue to be built in East Albany, development standards can encourage new development that faces these green corridors and activates them with trails and pocket parks, instead of ‘walling’ them off with rear yard fences, which can contribute to making these areas feel more privatized and inaccessible. As shown in the demonstration plan in Figure 6, these public corridors will create an identity for East Albany as a community with generous, healthy open spaces, integrated neighborhoods, and connections to nature.



Figure 7. East Albany Trails and Pathways



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New development facing a restored creek greenway, with natural play areas and trails providing a visual amenity for neighbors



Example of a townhome development integrated with adjacent public open space.



Example in North Bethany Oregon, of new development facing a restored creek greenway, with a bridge crossing and trails providing connectivity between several neighborhoods.



Existing Timber Linn Park, at the center of the study area.

## Other Parks and Gathering Spaces

In addition to creating trails, greenways and linear parks, the City will need to continue to develop other parks and recreation facilities to serve future new residents and workers in the area. The City's Parks Master Plan (PMP) guides development of these facilities, and identifies a variety of city-wide park and recreation needs and opportunities, which also apply in East Albany. These include:

- **Link Parks and Community Destinations with Trails.** Off-street trails and enhanced sidewalk connections will build loops and connections between parks, schools, downtown Albany, and the riverfront.
- **Support Health, Sport, and Fitness.** New park locations are opportunities to add sports fields and practice space, which are an identified need in Albany.
- **Coordinate Planning over the Long-Term.** Implementation of the PMP will require coordination with several City initiatives, the City's Transportation System Plan (TSP) and an Americans with Disabilities Act (ADA) Transition Plan to ensure the success of the park system.



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In addition, the Park Master Plan identifies priority types of facilities and activities to focus on within new parks or enhancements to existing parks.

- Make play area enhancements
- Add new amenities
- Develop new parks
- Renaturalize areas
- Develop additional sport courts

*Figure 8. Top Five Types of Recommended Park Improvements, 2021 Albany Parks Master Plan*



These strategies and recommendations should be implemented in East Albany as new areas are developed with housing and employment and as parks and recreation facilities are developed to serve them. In addition, the City should encourage development of public and private spaces for people to gather as part of development of residential and mixed use areas. These may include small outdoor seating areas, public plazas, benches and other amenities associated with new trails and greenways, and spaces which can be used for events and activities.

The East Albany Plan has not identified specific locations for individual parks or the number of parks that are likely to be needed in the future. However, the levels of service identified in the City's Park and Recreation System Plan can guide the City and development partners in determining appropriate park needs in newly developing areas. Additional action-planning strategies are found in Chapter 7 of this Plan and in the Parks Master Plan.



# Chapter 5

## Multimodal Transportation

This plan envisions a connected transportation system that provides safe, convenient, and efficient travel for all road users. Major roadways will deliver passengers and freight, while neighborhood routes and off-street trails will provide bicycle and pedestrian connectivity throughout the area and links to the rest of the City.

### Interstate 5 and Interchange Upgrades

The Oregon Department of Transportation (ODOT) has identified the need for changes to the interchange at I-5 and Knox Butte in order to reduce congestion and improve safety along the I-5 corridor. While updates to the interchange have not been designed in detail, and funding for the improvements has not yet been identified, several changes to the surrounding street system will be needed to accommodate eventual buildout.

### Arterial Streets

Arterials represent the highest class of city street. These roadways are intended to serve higher volumes of traffic, particularly through higher speeds. US Highway 20 (also known as Santiam Highway) is a principal arterial in East Albany, connecting to Downtown Albany and locations beyond the city. Goldfish Farm Road, Scrael Hill, Knox Butte, Three Lakes, and Grand Prairie Rd. are classified as minor arterials.

Figure 10 depicts the recommended cross-section of US-20, with sidewalks, planter strips, and a buffered bicycle lane in each direction, in addition to automobile travel lanes and a center turn lane. This design will provide greater safety, access, and comfort for pedestrians and people riding bicycles. The details of the design will vary depending on location, and will need to be agreed to by ODOT.

### Collector and Local Streets

The rest of the street network in East Albany will be made up of collector and local streets, which prioritize access to specific properties and comfortable travel over the quick movement of people and goods. Collector streets include Timber Ridge and Price Road (major collectors); and Sagecrest and Clover Ridge (minor collectors). The proposed cross section of the extension of Timber Ridge is shown in Figure 11. New east-west connections are proposed (likely as minor collectors) and depicted in Figure 12 through Figure 14.

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## New Roadway Connections

A number of new roads are recommended to ensure connectivity throughout the area as development occurs and to mitigate impacts related to planned future changes to Interstate 5 interchanges. These new connections are shown as dashed lines in Figure 9 and generally include the following:

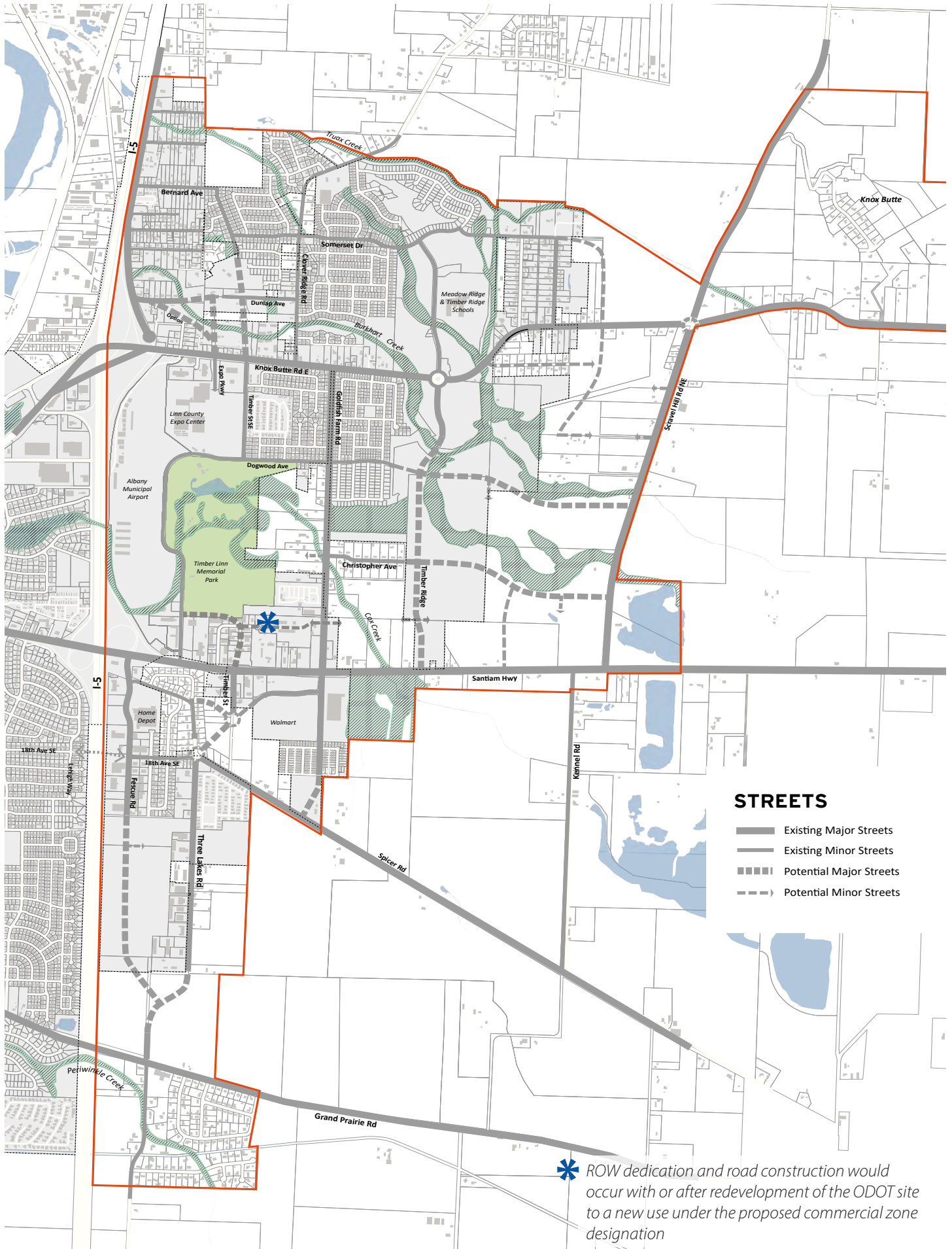
- New connections in the vicinity of Spicer Road and Maple Leaf Avenue: These connections have been identified in the City's Transportation System Plan (TSP) for a number of years and will be necessary in the long-term to address connectivity and mobility goals and issues.
- Connections north of Highway 20 in the vicinity of Timber Street, E Commercial Way and Blue Ox Drive: In the case of the east-west connection between E Commercial Way and Goldfish Farm Road, dedication and road construction would occur with or after redevelopment of the ODOT-owned site to a new use under the proposed commercial zone designation.
- New north-south and east-west connections east of Goldfish Farm Road would be implemented as new growth and development occurs in this area to ensure a well-connected street and pathway system is created.

Implementation of all new road connections will require further coordination with property owners and residents and will be addressed further in updates to the City's TSP.





Figure 9. East Albany Street Network



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### Roadway Cross Sections

The following illustrations depict typical conditions of roadways along various routes in East Albany, though there is flexibility on the ultimate design based on input from the City Engineer, ODOT, and others as appropriate.

Figure 10. Conceptual Cross Section - US Highway 20

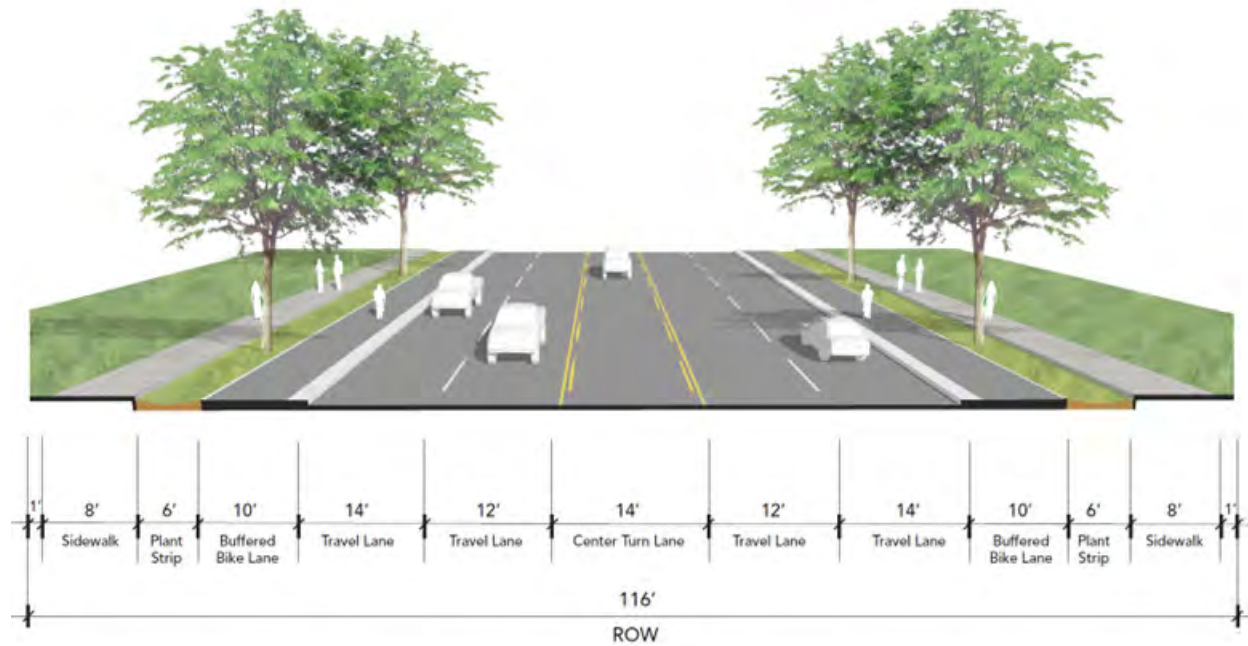
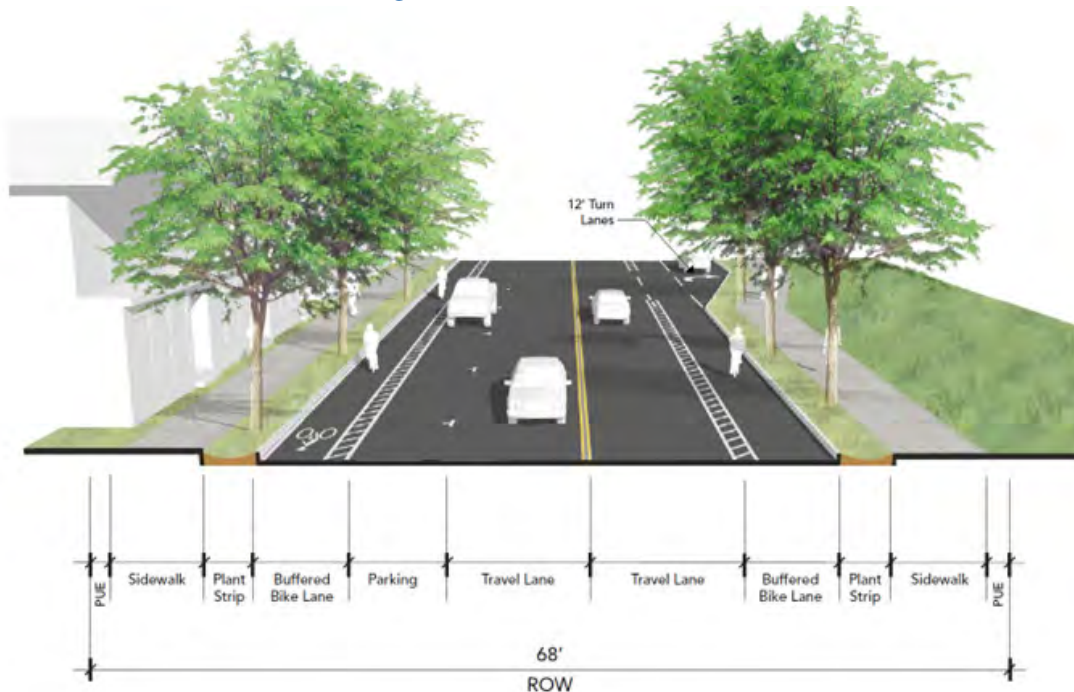


Figure 11. Cross Section - Timber Ridge Extension



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Figure 12. Cross Section - East West Connector (low traffic volume)

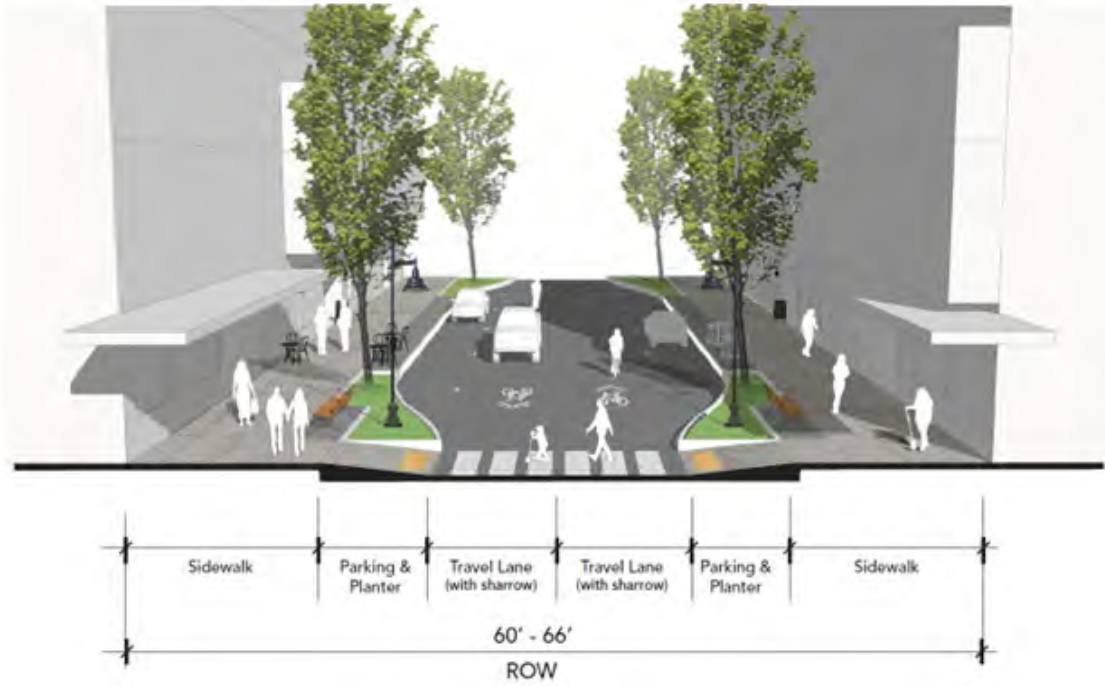


Figure 13. Cross Section - East West Connector (low traffic volume, open space edge)

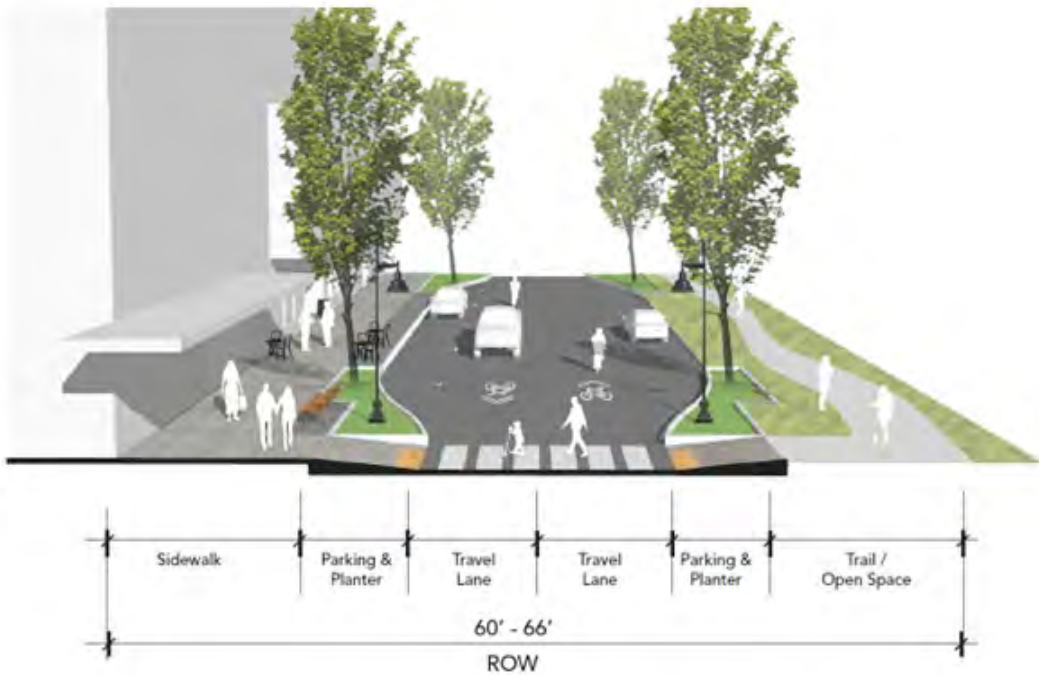
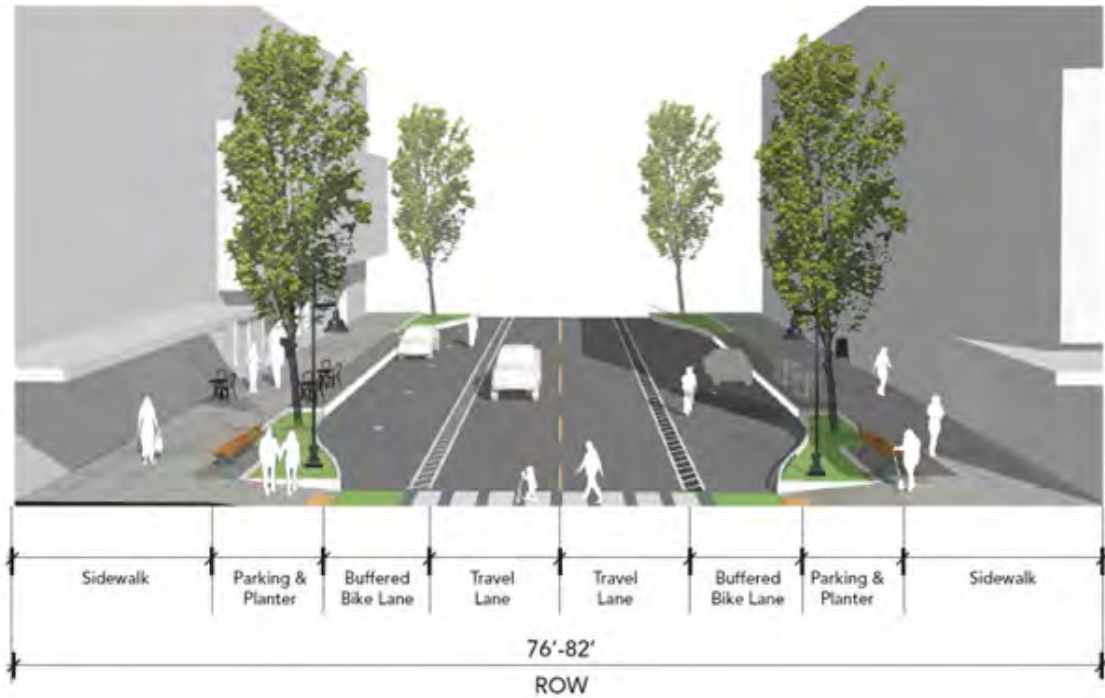






Figure 14. Cross Section - East West Connector (high traffic volume)



### Trails, Paths, and Bicycle Lanes

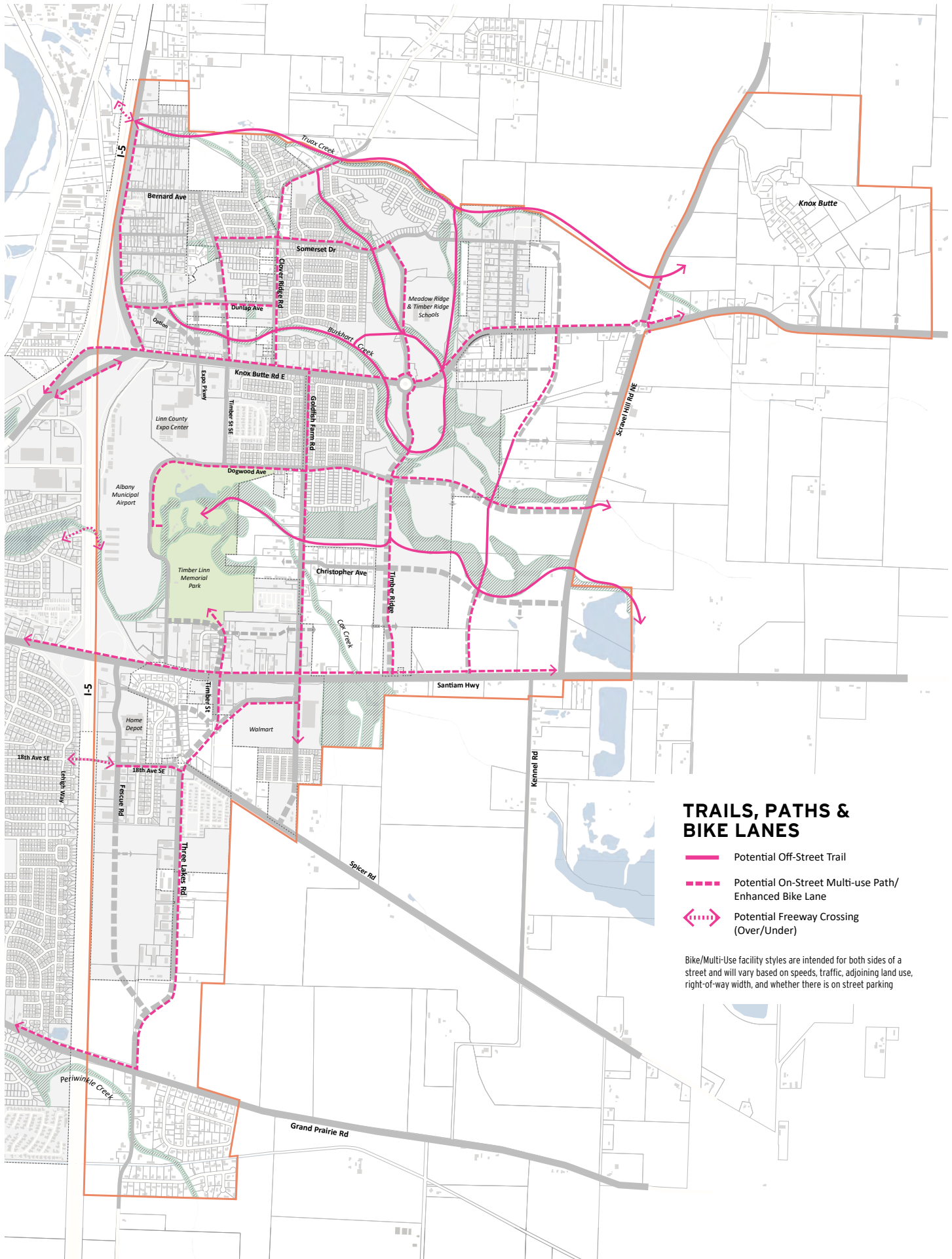
Figure 7 depicts the proposed system of on-street and off-street trails, bicycle lanes, and other features that will form a pedestrian- and bicycle-friendly network in East Albany. New connections will be made per this diagram and the City’s Transportation System Plan. As development occurs, county roads will be upgraded to City standards, complete with sidewalks and bicycle facilities.

This trail system has been designed to take advantage of the waterways and riparian areas that run through East Albany, allowing for public access to natural areas while preserving natural conditions and the ecosystem services (such as storm water management) that these greenways provide.



*Greenways will include trails, paths, protected bike lanes, and other safe connections between schools, neighborhoods, and nature.*

Figure 15. East Albany Trails and Pathways





# Chapter 6

## Infrastructure

This chapter details the major infrastructure improvements needed to meet the needs of future development in East Albany as described in this Plan, including the City of Albany water system, stormwater system, and sanitary sewer system. The City's public works department maintains master plans for these infrastructure systems, which will be updated in the future to include the information and proposed improvements identified in this chapter. In addition to the improvements summarized in this chapter, other localized water, sewer and stormwater management facilities will be needed and constructed as part of local development processes. These include but are not limited to smaller water, sewer and stormwater management facilities constructed in conjunction with local streets developed as part of future residential subdivisions or other individual developments.

Additional information related to implementation and funding for infrastructure is included in Chapter 7.

### Water System

Future water infrastructure needed for the East Albany area primarily include development-driven transmission/distribution projects, the Knox Butte Reservoir Project Phase 1, and the Knox Butte Reservoir Project Phase 2. Completed projects were updated based on City input. Additional mainline distribution piping is included for areas not previously evaluated by the City and to maintain looping in the system. Future mainlines should be located in major rights-of-way whenever possible. Figure 16 shows future water infrastructure in the East Albany study area. Table 2 shows projected linear feet of each pipeline size and costs for future pipelines and reservoirs.

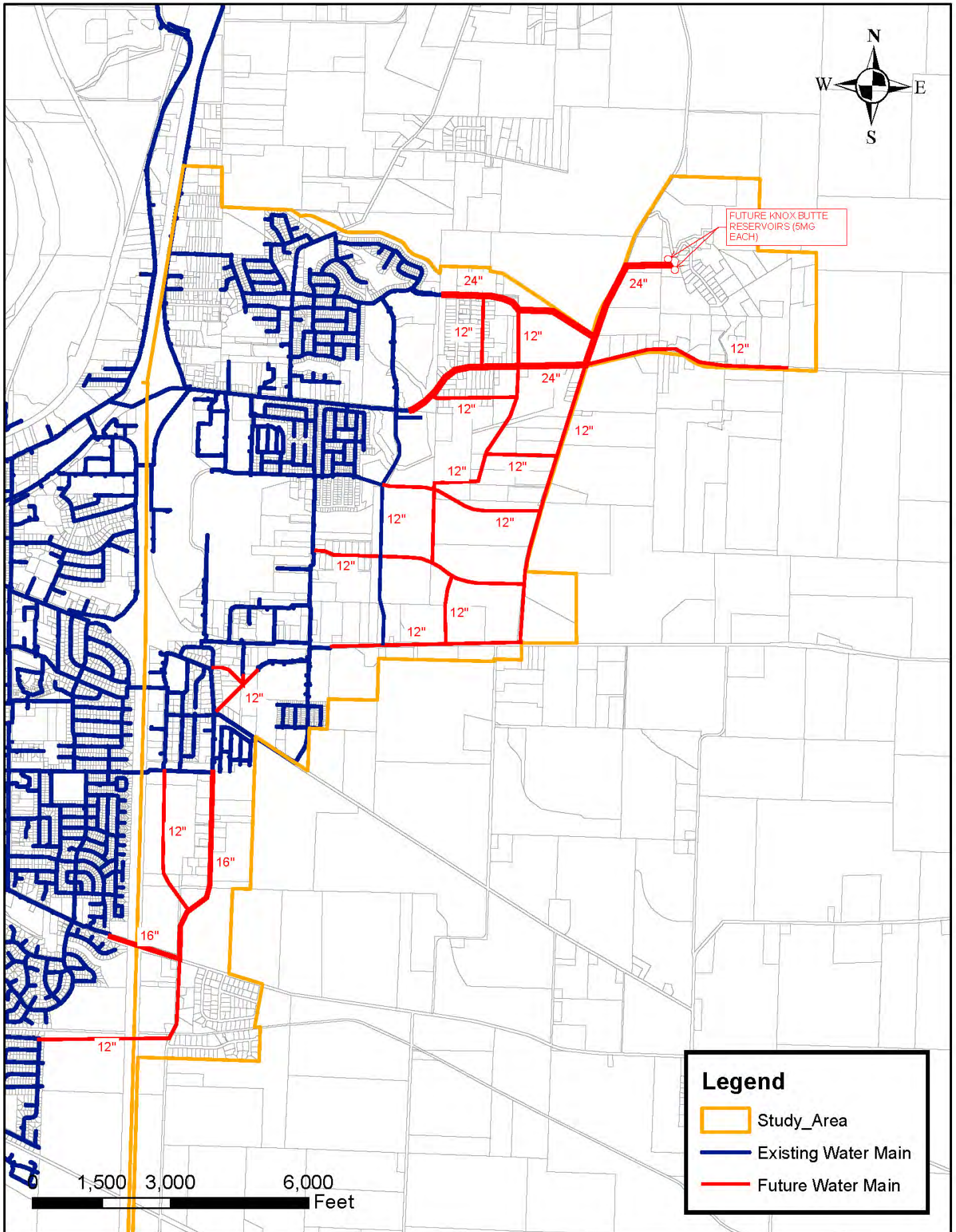
*Table 2. East Albany Study Area Water Cost Estimate*

General Improvement	Estimated Quantity	Unit	Total Cost
12-inch Water Pipe	50,343	LF	\$ 32,700,000
16-inch Water Pipe	6,000	LF	\$ 4,800,000
24-inch Water Pipe	12,580	LF	\$ 13,300,000
Knox Butte Reservoir 1	1	LS	\$ 20,600,000
Knox Butte Reservoir 2	1	LS	\$ 19,600,000
<b>Total:</b>			<b>\$ 91,000,000</b>

\* Unit cost estimates are based on current (2022) construction cost data from recent Keller Associates projects.



Figure 16. Water Infrastructure for East Albany





## Stormwater System

The East Albany study area consists of the entire Burkhardt-Truax Basin, and parts of the Cox Creek and Periwinkle basins (Figure 17). The 2021 Stormwater Master Plan (SWMP) identifies future stormwater infrastructure for the area. Assumed future stormwater infrastructure for this analysis comes from the 2021 Stormwater Master Plan (SWMP).

Figure 18 shows future storm infrastructure in the East Albany study area. Table 3 shows estimated planning level costs for future sewer infrastructure by basin. A more detailed breakdown of projects in each basin can be found in the SWMP.

*Table 3. East Albany Study Area Storm Cost Estimate*

General Improvement	Total Cost
Burkhart-Truax Basin	\$ 7,600,000
Cox Creek Basin	\$ 6,500,000
Periwinkle Basin	\$ 1,800,000
Albany Transportation System Plan - Stormwater Infrastructure	\$ 36,100,000
<b>Total:</b>	<b>\$ 52,000,000</b>

\* Unit cost estimates are updated from 2021 Avg. to current (January 2023) construction cost data using Engineering News-Record cost index 20-city average.

*Figure 17. City of Albany Storm Basins*

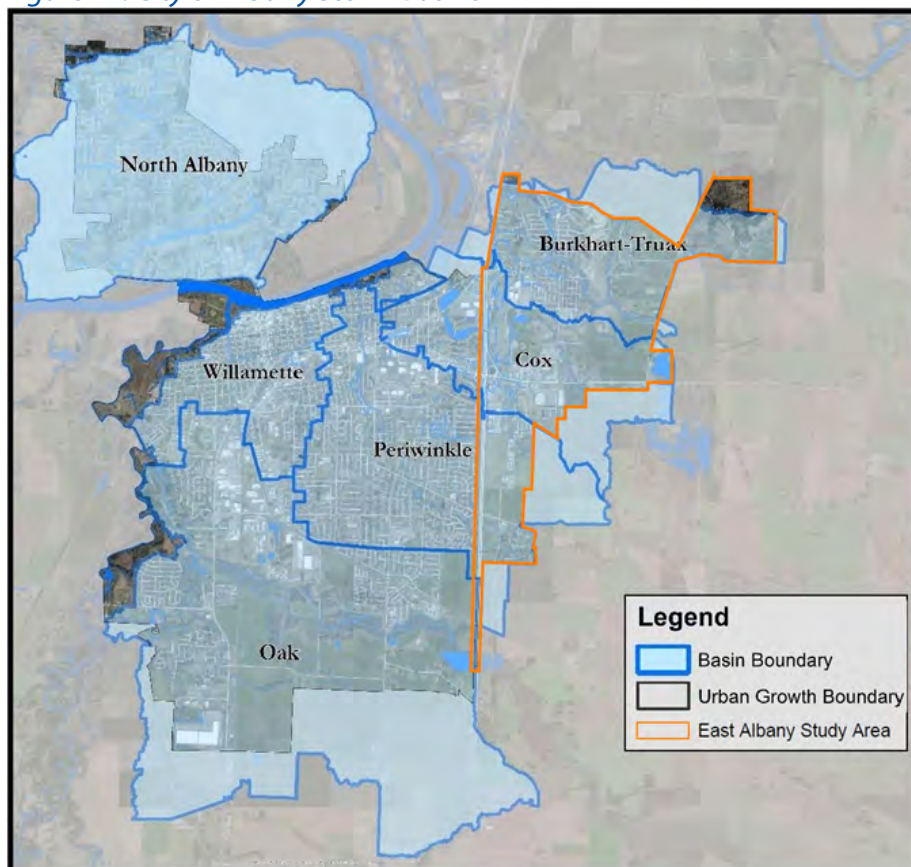
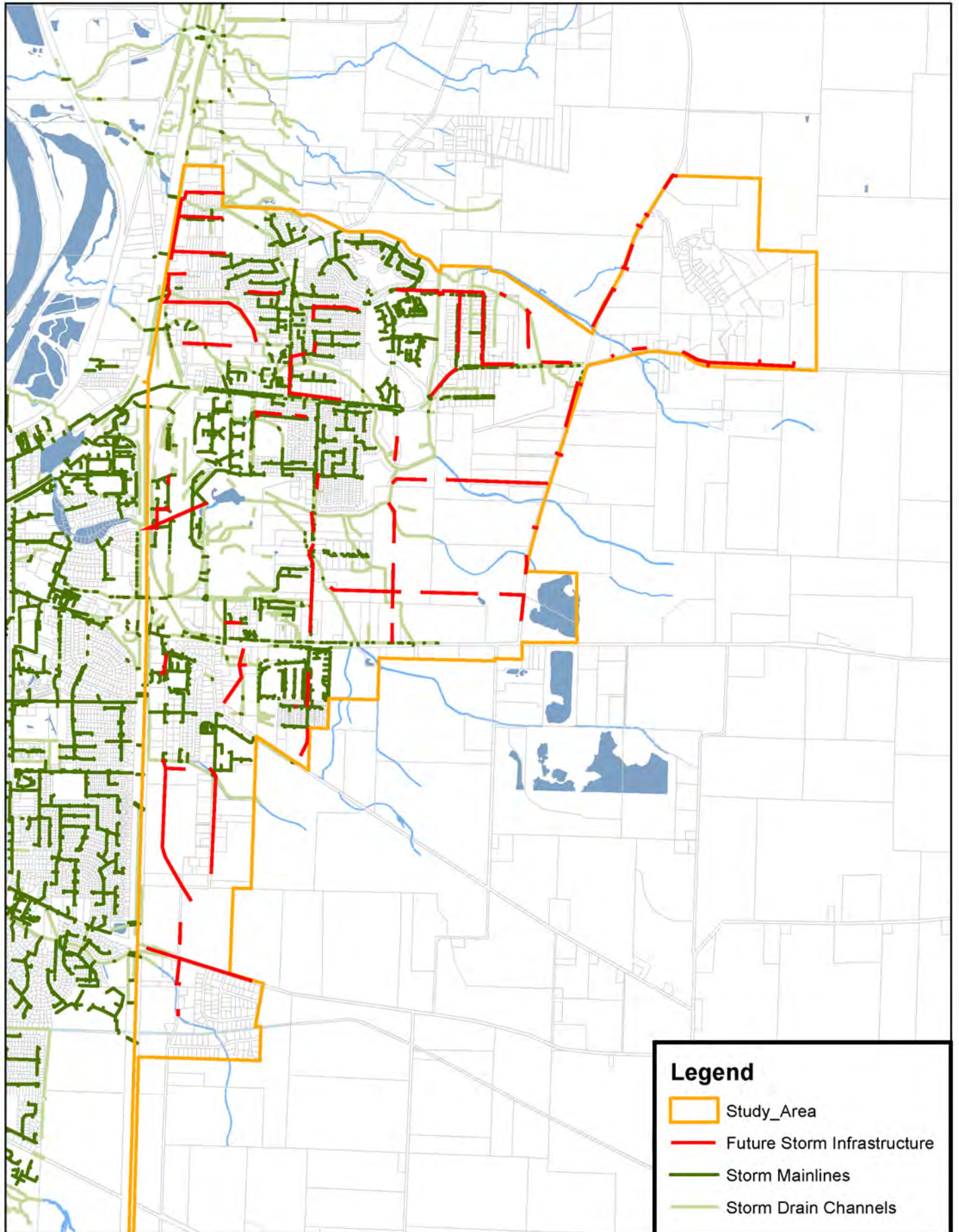




Figure 18. Storm Infrastructure for East Albany





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## Sanitary Sewer System

The City of Albany's 2015 Wastewater Collection System Facility Plan includes several projects in the East Albany study area. Most of these projects are intended to handle peak buildout flows during a 5-year storm event. The increased residential development projected in this Plan will increase base flows for the study area but should not have a major impact on existing or planned infrastructure. The list below summarizes Capital Improvement Projects called for in the Master Plan.

- Cox Creek Interceptor Projects
- Century Drive – Draperville Projects
- Somerset Drive Projects
- Three Lakes Road Projects
- Highway 20 Projects
- Timber Linn Projects
- Knox Butte Roads Projects
- Burkhart Creek Lift Station

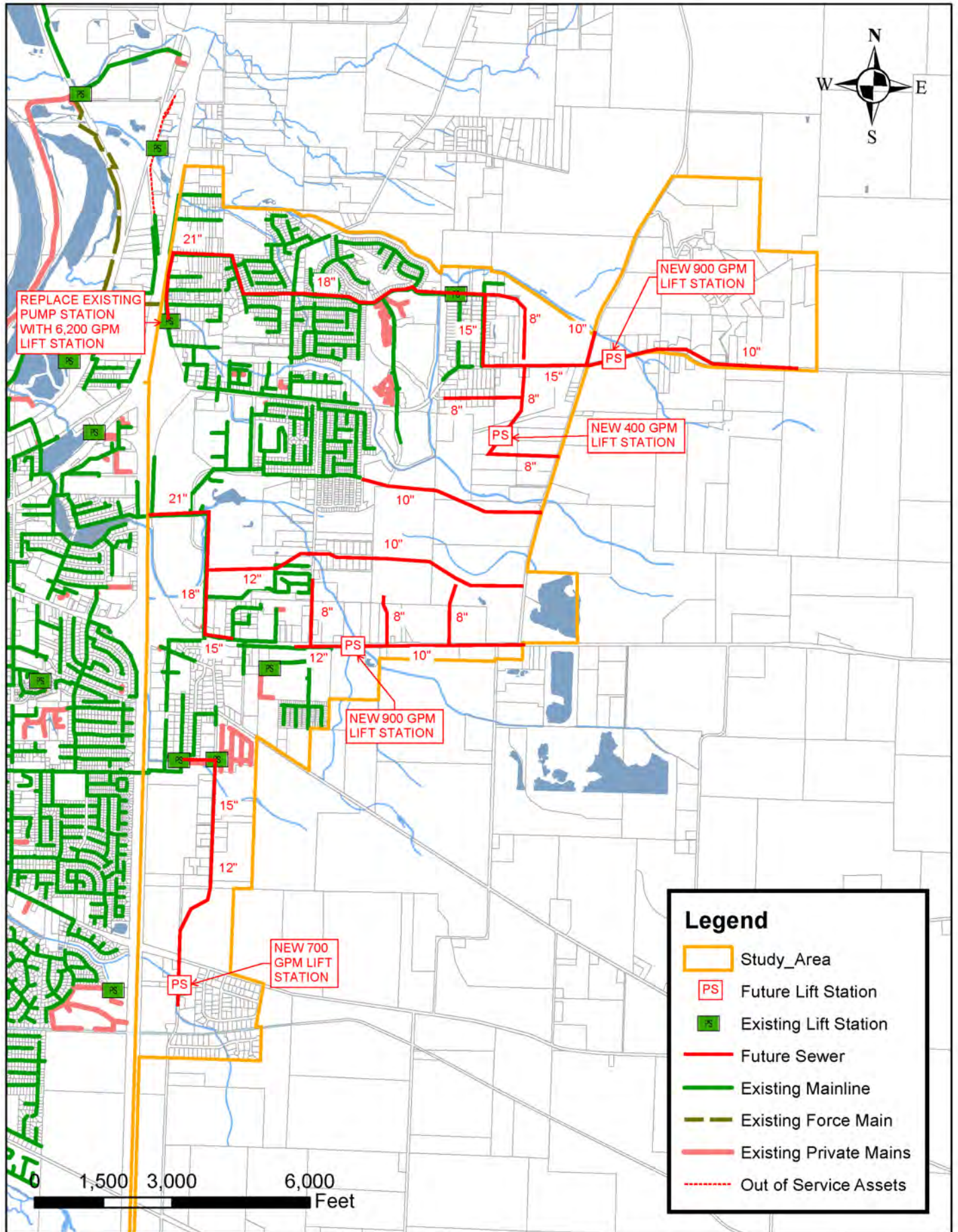
Pipeline and lift station improvements for these projects are embedded into the estimated costs shown in Table 4. Additional sewer mainline pipes not evaluated as part of the Facility Master Plan were added based on future street layouts and zoning identified in this Plan. Figure 19 shows future sewer infrastructure in the East Albany study area.

*Table 4. East Albany Study Area Sewer Cost Estimate*

General Improvement	Estimated Quantity	Unit	Total Cost
8-inch Sewer Pipe	10,930	LF	\$ 4,700,000
10-inch Sewer Pipe	18,090	LF	\$ 8,500,000
12-inch Sewer Pipe	6,300	LF	\$ 3,000,000
15-inch Sewer Pipe	10,850	LF	\$ 6,100,000
18-inch Sewer Pipe	5,775	LF	\$ 3,500,000
21-inch Sewer Pipe	4,125	LF	\$ 2,500,000
400 GPM Lift Station	1	EA	\$ 1,700,000
700 GPM Lift Station	1	EA	\$ 2,400,000
900 GPM Lift Station	1	EA	\$ 2,700,000
900 GPM Lift Station	1	EA	\$ 2,700,000
6200 GPM Lift Station	1	EA	\$ 12,800,000
<b>Total:</b>			<b>\$ 50,600,000</b>

\* Unit cost estimates are updated from January 2021 to current (January 2023) construction cost data using Engineering News-Record cost index 20-city average.

Figure 19. Sewer Infrastructure for East Albany





# Chapter 7

## Implementation

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The East Albany Plan will be implemented over time through the actions of the City of Albany, its partnering agencies and jurisdictions, and private property owners. This chapter describes the overall process for public improvements, annexation, development review, and other actions needed to make the East Albany Plan vision a reality.

### Annexation and Development

Much of the land in the East Albany planning area is currently outside the Albany city limits. Over time, those areas are expected to be annexed into the City. This will occur in the following phases:

- **Phase 1: Island Annexations.** The City plans to work with property owners in existing “islands” (areas outside the city limits but surrounded by areas within the limits) to encourage and support them in annexing their properties, if desired. This process is planned to occur within one to two years after adoption of the East Albany Plan.
- **Phase 2: Additional Annexations.** The City also will support requests and applications for annexation from other property owners in East Albany as the City continues to grow and develop and as those property owners are interested in bringing their properties into the City limits. At the time those properties are annexed, the City will apply zoning designations to them, consistent with the Comprehensive Plan map and the East Albany land use plan.

### Comprehensive Plan Map Updates

The East Albany Plan identifies amendments to the City’s comprehensive plan map to achieve the goals of the plan. The most significant change is replacing the “Urban Residential Reserve” designation with a planned program of residential, employment, and mixed use designations to provide greater certainty to property owners and to inform transportation and utility investment by the City. These changes are detailed in Appendix B.



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## Comprehensive Plan Text Updates

The proposed land use and zoning designations within the East Albany Plan area require amendments to the Land Use Planning chapter of the Comprehensive Plan (Chapter 9). Proposed amendments include the following:

- **Reinstitute the Residential High Density designation.** This designation was removed from the Comprehensive Plan in 2007; however, as noted above, the existing residential designations are not adequate for the high-density residential uses that are recommended for the East Albany area.
- **Add a new Employment designation.** This would be similar to the Light Industrial designation, but would allow a broader range of office and professional uses. Potential implementing zones include the new Employment (EMP) zone, as well as Industrial Park (IP), and Office Professional (OP).
- **Add a sub-category within the Village Center designation: Village East.** The Village East designation would only apply in East Albany and could only be implemented by the Mixed Use Commercial (MUC) and Mixed Use Residential (MUR) zoning districts.
- **Rename the Commercial Light designation: Neighborhood Village and add the MUR zone.** This designation is applied in areas throughout the City, and is intended for neighborhood-scale commercial and residential uses. This designation could be implemented by the Neighborhood Commercial (NC), Office Professional (OP), and MUR zoning districts.
- **Update the Plan Designation Zoning Matrix.** This table will be updated with the proposed new Comprehensive Plan designations and compatible zoning districts listed above.

Additionally, a new section within the Urbanization chapter of the Comprehensive Plan (Chapter 8) is also proposed to further the vision, goals, and objectives for the East Albany Plan area. This section is modeled after existing sections for the North Albany and South Albany areas. Much of the proposed goal and policy language for the new East Albany section is derived from earlier work in developing the East Albany Plan, including the Vision and Goals memo, Gap Analysis, and Development Alternatives report. Policies are proposed for the following topics: Land Use, Transportation, Economic Development, Housing, Natural Resources and Greenways, Aesthetic and Recreational Amenities, and Infrastructure.

Amendments to the Albany Development Code are needed to add the new High Density Residential and Employment zones and update the Plan Designation Zoning Matrix.



## Zoning

Zoning is applied to land within Albany's city limits. A few zoning map amendments are included as part of the adoption of the East Albany plan, detailed in Appendix B of this document. Property owners can also undertake zoning amendments to conform with the underlying comprehensive plan designation as desired. For areas outside the city limits, annexation will require zoning be applied consistent with the comprehensive plan and the East Albany Plan.

## Funding Strategies and Sources

The extensive infrastructure improvements needed to serve the build out of the preferred EAP concept will carry high costs. These improvements are likely to be completed in phases over the planning period, allowing for phased funding as well. This section briefly describes potential funding sources for public improvements. More information about these strategies is found in Appendix E.

### 1. System Development Charges

The City of Albany has established system development charges (SDCs) for water, sewer, parks, and transportation that apply to new development. The SDCs generated with extensive new development expected in the plan area have the potential to generate significant revenue for making additional improvements.

### 2. Tax Increment Financing

The Tax Increment Financing (TIF) mechanism can be a powerful tool for generating dedicated funding for making public improvements within an identified district. TIF works by "freezing" the current property tax base in the TIF district and assigning the future tax growth to the district itself to pursue projects identified in an adopted Plan.

Urban Renewal funds must generally be used for physical improvements to infrastructure and property, which may be public or private. A number of larger-scale transportation improvement projects in East Albany may lend themselves to the use of TIF funding because they serve a larger area and may be challenging to pay for through other mechanisms. These include some of the connectivity improvements in the Price Road area, improvements to existing roadways such as Knox Butte Road, Goldfish Farm Road, and off-road trail systems and paths.

TIF Districts must undergo a feasibility and planning process to determine the boundaries, projects, and revenue potential of the district. The TIF district must be adopted in a TIF plan and approved by the voters.

### 3. Local Improvement District

Improvement districts assign all or a portion of the cost of infrastructure improvements on the properties that will directly benefit from them. A local improvement district (LID) is a method for a group of property owners to pay for improvements that will provide collective benefits to them all.

Albany allows for LIDs created either by the property owners benefiting from an improvement, or by the City. The LID assessment may be paid at once by the property owner,

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or over 10 years with interest. The size of the East Albany study area could allow for multiple smaller LIDs focused on financing particular public improvements.

#### 4. Bond or Levy

**General Obligation Bond:** General Obligation (GO) bonds are secured by a taxing jurisdiction's ability to levy an increased property tax sufficient to pay the bond. The additional property tax is dedicated solely to repaying the bonds and cannot be used for other purposes.

GO bonds must be approved by a majority of voters and may only be issued to finance capital costs associated with the acquisition, construction, improvement, remodeling, furnishing, equipping, maintenance, or repairing of real or personal property.

**Local Option Levy:** A local option levy is a time-limited property tax (five years for operations and 10 years for capital projects), that is subject to voter approval. It is levied in addition to a taxing jurisdiction's permanent rate to pay for specified programs or investments. Local option levies are issued as a rate, rather than an amount, meaning that actual revenues may fluctuate from year to year with new development. Levies may be used for programs or operations, in addition to capital projects.

#### 5. Construction Excise Tax (for Affordable Housing)

This tool may be used to achieve new development in the study area if it includes affordable housing. The construction excise tax (CET) is a tax on construction activity of new structures or additional square footage to an existing structure in order to provide a source of funding used to incentivize housing affordable at 80% of AMI or less. Cities or counties may levy a CET on residential construction of up to 1% of the permit value, or on commercial and industrial construction with no limit on the rate.

This tool is currently under consideration in Albany as part of the Albany Housing Implementation Plan project.

#### 6. Development Incentives and Tools

Market-based strategies can provide incentives to encourage developers to build desired types of projects in the community. In general, these incentives help to reduce some of the costs of development. While the bulk of development costs are set by private market labor and materials costs, these steps can provide incentives on the margin to facilitate development.

##### *SDC or Fee Reductions or Deferrals*

Reduction, exemption, or deferral of SDCs or development fees directly reduces the soft costs of development to applicants for desired development types.

##### *Tax Exemptions and Abatements*

Tax exemptions or abatements offer another financial incentive to developers that can improve the long-term economic performance of a property and improve its viability. This





can be a substantial incentive, but the City and/or County will forego taxes on the property, generally for ten years. Other taxing jurisdictions are not included unless they agree to participate.

*Additional Implementation Actions and Timeline*

A variety of additional actions and strategies will need to be undertaken over time to implement this Plan. Many of these strategies were identified in the “Gap Analysis” prepared earlier in the planning process. The following summary of actions, lead and partnering entities, and general timelines for implementation builds on that work.

*Table 5. East Albany Plan Implementation Actions*

Action	Lead Responsibility	Timeline
<b>Land Use Planning and Development</b>		
Rezone land within City limits consistent with EAP land use plan	Albany Community Devel. Dept. (CDD)	Short-Term
Apply new comprehensive plan designations outside City limits	CDD	Short-Term
Update Albany Development Code to adopt new zoning designations and additional requirements related to allowed uses and development standards	CDD	Short-Term, Ongoing
Evaluate and process annexation requests	CDD	Short-Term, Ongoing
Upon annexation, apply zoning designations, consistent with EAP land use plan	CDD	Ongoing
Implement recommendations from the Albany Housing Implementation Plan to encourage, require or support development of needed housing types	CDD; housing, development partners	Medium to Long-Term
<b>Transportation Facilities Planning and Development</b>		
Identify needed amendments to the City’s Transportation System Plan	Albany Public Works Dept. (PW)	Short-Term
Continue to coordinate with property owners regarding implementation of new road connections as development and annexation occur.	CDD, PW	Medium to Long-Term
Estimate planning level costs of needed transportation improvements	Consultants, PW	Short-Term, Refine Medium to Long-Term



Action	Lead Responsibility	Timeline
Update the City's transportation facilities capital improvements plan to include East Albany projects	PW	Short-Term
Amend Albany Development Code, as needed to require multimodal connectivity in new developments	PW	Short-Term
Engage state representatives on state and federal infrastructure funding	PW	Ongoing
Partner with Linn County in designing and improving County roads and bringing them up to City standards	PW	Ongoing
<b>Transportation Facilities Planning and Development</b>		
Continue to work with Albany Transit, the MPO, and regional providers to achieve enhanced transit service for East Albany residents and employees and to better serve large events and facilities	PW, CDD	Ongoing
Pursue public/private partnerships to address transportation and other infrastructure improvements to address transportation, other impacts	PW, CDD	Ongoing
Identify specific projects that could be funded through alternative strategies such as a new Tax Increment Financing District, Local Improvement District(s), and/or Bond Measures	PW, CDD	Short to Medium-Term
<b>Parks and Trails Facility Planning and Development</b>		
Evaluate requirements for providing common space required with development; refine as needed	CDD	
Update the City's Parks and Recreation System plan and project list to include needed and proposed park and trail projects in East Albany	Albany Parks & Recreation Dept. (Parks)	Short to Medium-Term
Identify appropriate funding sources and partners for specific improvements, including SDCs, other development funding strategies, public-private partnerships, grants, etc.	Parks	Short to Medium-Term



Action	Lead Responsibility	Timeline
Update the City's Parks SDC to include new projects in the CIP and changes to the SDC rate and fees, as needed	Parks	Medium-Term
Provide incentives (density transfer, credits) for developments that implement protections of natural resource and open spaces areas	CDD, Parks	Medium to Long-Term
Continue to update the ADC to refine requirements for protections/open spaces	CDD	Medium to Long-Term
<b>Parks and Trails Facility Planning and Development</b>		
Continue to acquire open space land through development or prior to development to build a greenway system	Parks	Ongoing
Conduct detailed design processes for individual parks and trail projects; conduct robust community engagement processes as part of those efforts	Parks	Medium to Long-Term
<b>Water, Wastewater, and Stormwater Planning and Facility Design and Construction</b>		
Update water, wastewater, and stormwater facility master plans to reflect needed projects identified in this Plan; incorporate cost estimates in updated Capital Improvement Plans (CIPS)	PW	Short to Medium-Term
Continue to address deficiencies already identified in facility Master Plans	PW	Ongoing
Update SDC CIPs and methodologies, including rates and fees to reflect updated master plans	PW	Medium to Long-Term
Continue to require that local infrastructure be constructed as part of the land development process	PW	Ongoing
<b>Economic Development and Employment</b>		
Identify strategic initiatives for developing and/or reducing barriers to development of large parcels – such as Local Improvement Districts, tax increment financing district	Albany Economic Development Division (EDD), partners	Short to Medium-Term





Action	Lead Responsibility	Timeline
Establish or partner to create Citywide/regional wetland mitigation programs, especially those in the study area that might enhance riparian corridors and a greenway system	City, Cascades West Regional Wetlands Consortium	Medium to Long-Term
Conduct City-initiated site preparation associated with any identified public-private development partnerships	EDD	Medium to Long-Term
Continue to implement economic development strategies identified in the City's Economic Opportunities Analysis	EDD	Ongoing
<b>Economic Development and Employment</b>		
Market employment sites through a variety of local, regional and statewide marketing and promotion efforts	EDD, Chamber, other partners	Ongoing
Seek state and federal funding for infrastructure and/or other improvements that will contribute to site readiness	EDD	Ongoing
Support annexation and rezoning of commercial and employment properties, consistent with this Plan	EDD, CDD	Medium to Long-Term
Support future development proposals that are consistent with this Plan	EDD, CDD	Ongoing
<b>Schools and Other Public Services</b>		
Support efforts by the Greater Albany Public School District (GAPS) to continue to expand existing schools and construct new schools to meet the needs of current and future East Albany residents and families	CDD; Partner: GAPS	Medium to Long-Term
Support the GAPS in identifying sites for new school facilities	CDD; Partner: GAPS	Medium to Long-Term
Work with the GAPS to review plans and land use applications for school facility projects	CDD; Partner: GAPS	Ongoing
Support efforts by the GAPS to promote greater levels of walking and bicycling by students and their families to access schools in East Albany	CDD and PW; Partner: GAPS	Ongoing
Continue to plan for needed emergency service and law enforcement facilities in East Albany	Albany Fire & Police Depts.	Medium to Long-Term

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Action	Lead Responsibility	Timeline
<b>Other Goals and Initiatives</b>		
Conduct design competition for local artists to design gateway features	CDD; Partner: arts groups	Medium to Long-Term
Seek City or state grant funding for gateway improvements	CDD	Medium to Long-Term
Secure employer or business sponsorship or contributions for gateway and/or other public art projects	CDD; Partner: local businesses, civic groups	Medium to Long-Term
Use art grants or programs to fund selected gateway projects and/or other public art projects	CDD	Medium to Long-Term